

SEE SHEET 3 OF 3 FOR NOTES

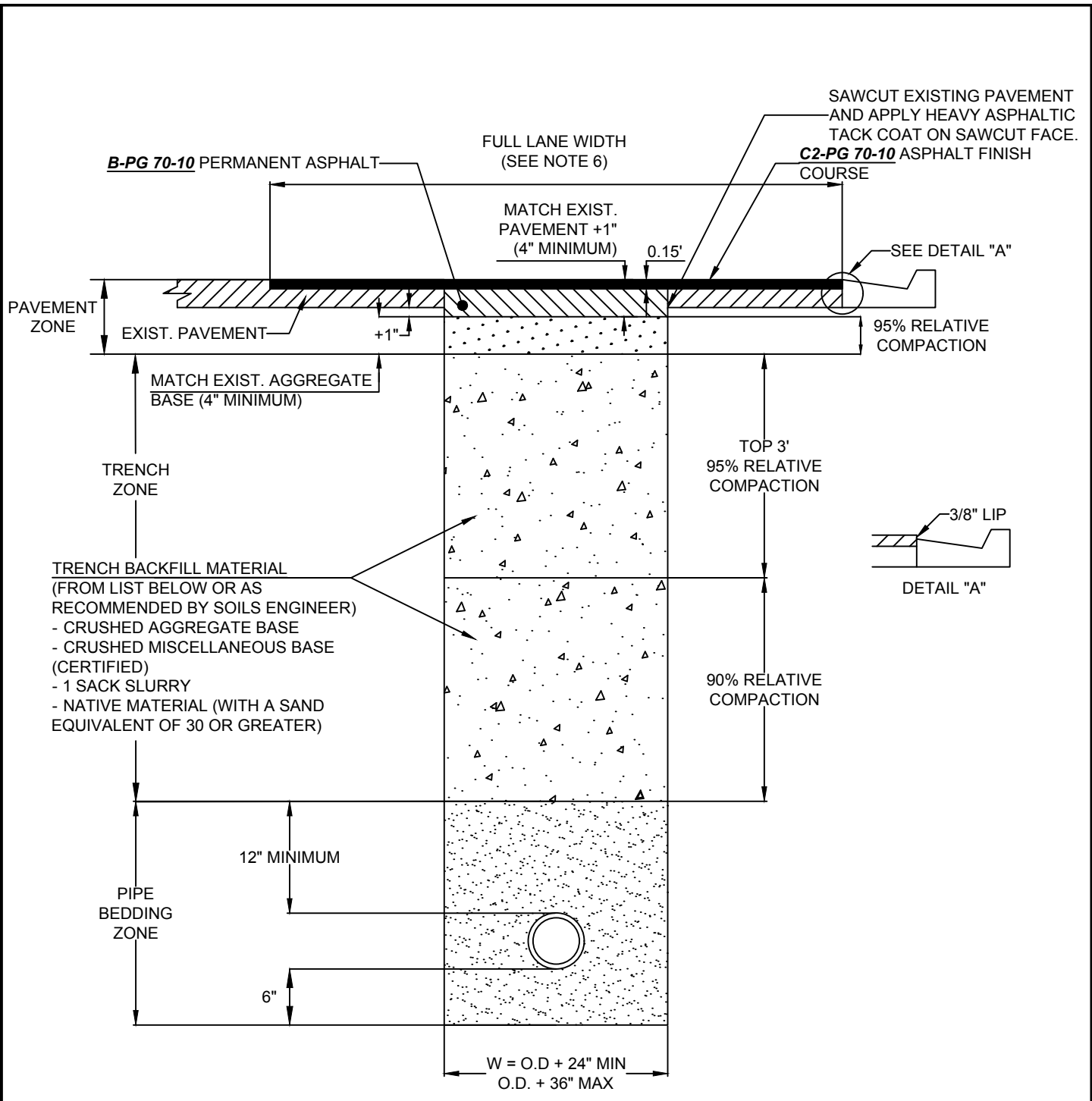
**TRENCH PERPENDICULAR TO CURB**

APPROVED BY:  
  
 8/6/2014  
 DIRECTOR OF PUBLIC WORKS  
 JONATHAN GEORGE SMITH



**CITY OF MENIFEE**  
**TRENCH BACKFILL AND ROADWAY REPAIR (CASE I)**  
 STANDARD PLAN NO. **812** SHEET **1** OF **4**

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SEE SHEET 3 OF 3 FOR NOTES

**TRENCH PARALLEL TO CURB**

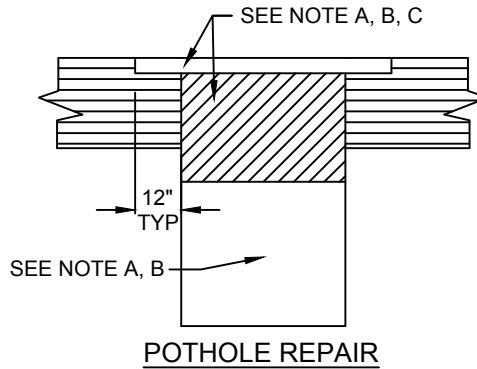
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**CITY OF MENIFEE**  
**TRENCH BACKFILL AND**  
**ROADWAY REPAIR**  
**(CASE II)**

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STANDARD PLAN NO. **812** SHEET **2** OF **4**



- A. POTHOLE BACKFILL MAY BE COMPLETED USING DEEP LIFT (FULL SECTION) AC FOR POTHOLES THAT ARE 2' x 2' IN AREA OR LESS.
- B. THE MATERIAL USED FOR BACKFILLING POTHOLES THAT ARE LARGER THAN 2' x 2' SHALL BE 95% COMPACTED CRUSHED AGGREGATE BASE (CAB) OR CMB AS APPROVED.
- C. POTHOLES SHALL BE CAPPED USING **C2-PG 70-10** PERMANENT ASPHALT WHICH IS 1" THICKER THAN THE EXISTING ASPHALT. IN ADDITION, ALL POTHOLES SHALL BE COLD MILLED 12" BEYOND ALL EDGES TO A DEPTH OF 0.1' AND CAPPED USING **C2-PG 70-10** PERMANENT ASPHALT.

**CITY OF MENIFEE REQUIREMENTS  
FOR TRENCHES OR OTHER EXCAVATIONS WITHIN  
PUBLIC RIGHTS-OF-WAY OR EASEMENTS**

**NOTES**

1. ALL TRENCH EXCAVATIONS SHALL BE COMPLETED BY FIRST SAW-CUTTING THE PAVEMENT BEFORE EXCAVATION. ALL SAW CUT LINES SHALL BE CLEAN AND FREE OF ROUGH EDGES. ADDITIONAL SAW-CUTTING WILL BE REQUIRED BY THE PUBLIC WORKS INSPECTOR IF THE EDGES OF THE TRENCH ARE DAMAGED DURING EXCAVATION OR BACKFILLING OPERATIONS.
2. ALL COMPACTION OF TRENCH BACKFILL MATERIAL SHALL BE ACCOMPLISHED BY MECHANICAL METHODS. JETTING, PONDING OR FLOODING IN LIEU OF MECHANICAL METHODS SHALL NOT BE ALLOWED.
3. ALL TRENCHES SHALL BE BACKFILLED AND A MINIMUM OF 3" OF TEMPORARY ASPHALT PAVEMENT INSTALLED AT THE END OF EACH WORKDAY. THE PUBLIC WORKS INSPECTOR MAY AUTHORIZE STEEL PLATE BRIDGING IN ACCORDANCE WITH STANDARD #813 IN LIEU OF TEMPORARY ASPHALT PAVEMENT.
4. ALL TRAFFIC STRIPING OR MARKINGS REMOVED OR DAMAGED DURING CONSTRUCTION SHALL BE REPLACED USING THERMOPLASTIC COATINGS OR AS DIRECTED BY THE PUBLIC WORKS INSPECTOR. PARTIALLY REMOVED STRIPING SHALL BE REPLACED IN WHOLE.
5. PERMANENT PAVEMENT REPAIR SHALL BE ACCOMPLISHED WITHIN 14 DAYS OF TEMPORARY REPAIR BY REMOVAL OF ALL TEMPORARY AC PAVEMENT, INSTALLATION OF PERMANENT ASPHALT PAVEMENT AS NOTED ON THIS STANDARD, AND COLD MILLING WITH FINISH PAVEMENT.
6. COLD MILLING OF 0.15' SHALL BE REQUIRED FOR ALL TRENCHES. THE PUBLIC WORKS INSPECTOR WILL REQUIRE ADDITIONAL COLD MILLING IF FIELD CONDITIONS SO WARRANT.
7. ADDITIONAL COLD MILLING SHALL BE REQUIRED FOR TRENCHES THAT ARE 2' TO 4' FROM THE CURB & GUTTER.
8. REMOVE AND REPLACE ASPHALT PAVEMENT FULL DEPTH FOR TRENCHES THAT ARE 2' OR LESS FROM CURB & GUTTER.
9. TACK COAT OF **PG 70-10** PAVING ASPHALT SHALL BE UNIFORMLY APPLIED TO EXISTING ASPHALT SURFACES PRECEDING PLACEMENT OF NEW ASPHALT CONCRETE. THE SURFACE SHALL BE FREE OF WATER, FOREIGN MATERIAL, OR DUST WHEN THE TACK COAT IS APPLIED.
10. FOR WATER AND SEWER PIPE BEDDING REQUIREMENTS REFER TO EASTERN WATER MUNICIPAL WATER DISTRICT (EMWD) STANDARDS.
11. TESTING: COMPACTION REPORTS SHALL BE SUBMITTED TO THE CITY ENGINEER.
12. PROHIBITION OF PAVEMENT CUTTING: ASPHALT CONCRETE PAVEMENT LESS THAN THREE (3) YEARS OLD SHALL NOT BE CUT EXCEPT FOR EMERGENCY REPAIRS OR AS SPECIFICALLY APPROVED IN WRITING BY THE CITY ENGINEER. SPECIAL REQUIREMENTS WILL BE IMPOSED FOR REPAVING.

APPROVED BY:

*Jonathan George Smith*

8/6/2014

DIRECTOR OF PUBLIC WORKS  
JONATHAN GEORGE SMITH

DATE



**CITY OF MENIFEE**

**TRENCH BACKFILL AND  
ROADWAY REPAIR**

STANDARD PLAN NO. **812** SHEET **3** OF **4**

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① 190-E-400 CONCRETE, MAXIMUM 8" SLUMP SLURRY BACKFILL TO SURFACE. FOLLOW WITH 12" WIDE BY 1-1/2" DEEP GRIND AND RESURFACING. ALLOW MIN. 72 HOURS CURE BEFORE GRINDING.

② C2-PG 70-10 ASPHALT CONCRETE.

③ ALL CONDUIT AND CABLE.

④ EXISTING A.C. PAVEMENT.

⑤ EXISTING BASE MATERIAL.

⑥ MORTAR SAND COMPACTED TO 95% RELATIVE DENSITY.

⑦ UNDISTURBED SOIL.

⑧ SYMMETRICAL ABOUT CENTERLINE OF TRENCH.

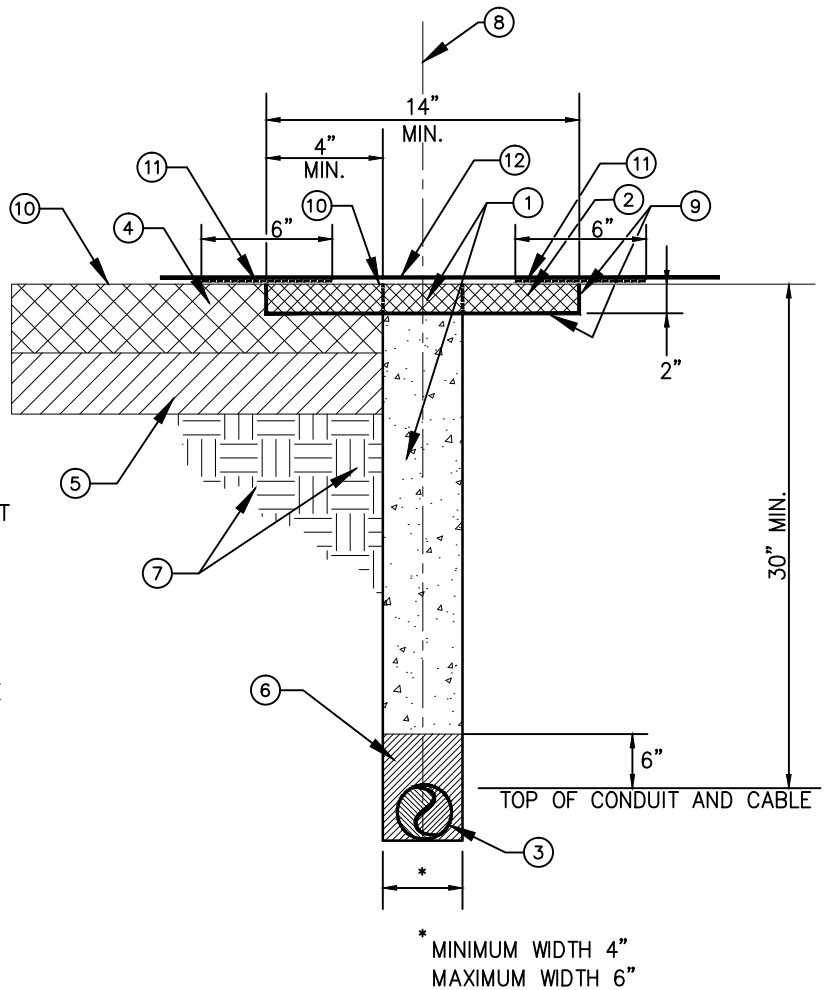
⑨ GRADE SS-1h EMULSIFIED ASPHALT APPLIED AT 0.15 GALLON PER SQUARE YARD.

⑩ EXISTING ASPHALT PAVEMENT FINISHED GRADE, SMOOTHNESS & COMPACTION OF RESURFACING SHALL MEET THE REQUIREMENTS OF SEC 302-5.6.2 SSPWC EXCEPT THAT THE SMOOTHNESS SHALL BE DETERMINED OVER THE LENGTH & WIDTH OF PAVEMENT AREAS DISTURBED BY THE CONTRACTOR'S/PERMITEE'S OPERATIONS.

⑪ RESPRAY GRADE SS-1h EMULSIFIED ASPHALT AT 0.15 GALLON PER SQUARE YARD 6" WIDE, CENTERED ON EDGE LINE OF GRIND AFTER PLACING A.C. & BEFORE SURFACE TREATMENT.

⑫ SURFACE TREATMENT TO MATCH EXISTING PAVEMENT (E.G. SEAL COAT, CHIP SEAL).

⑬ WHEN THE EDGE OF THE GRIND LINE IS WITHIN 12" OF EDGE OF PAVEMENT, ANY STRUCTURE, AN ADJACENT TRENCH PATCH OR ANY OTHER PAVING JOIN LINE, THE 1-1/2" DEEP GRIND SHALL BE EXTENDED TO THE EXISTING STRUCTURE OR JOIN LINE.



NARROW TRENCH BACKFILL & ASPHALT  
CONCRETE RESURFACING DETAIL  
N.T.S.

APPROVED BY:

*Jonathan George Smith*

8/15/2014

DIRECTOR OF PUBLIC WORKS  
JONATHAN GEORGE SMITH

DATE



CITY OF MENIFEE

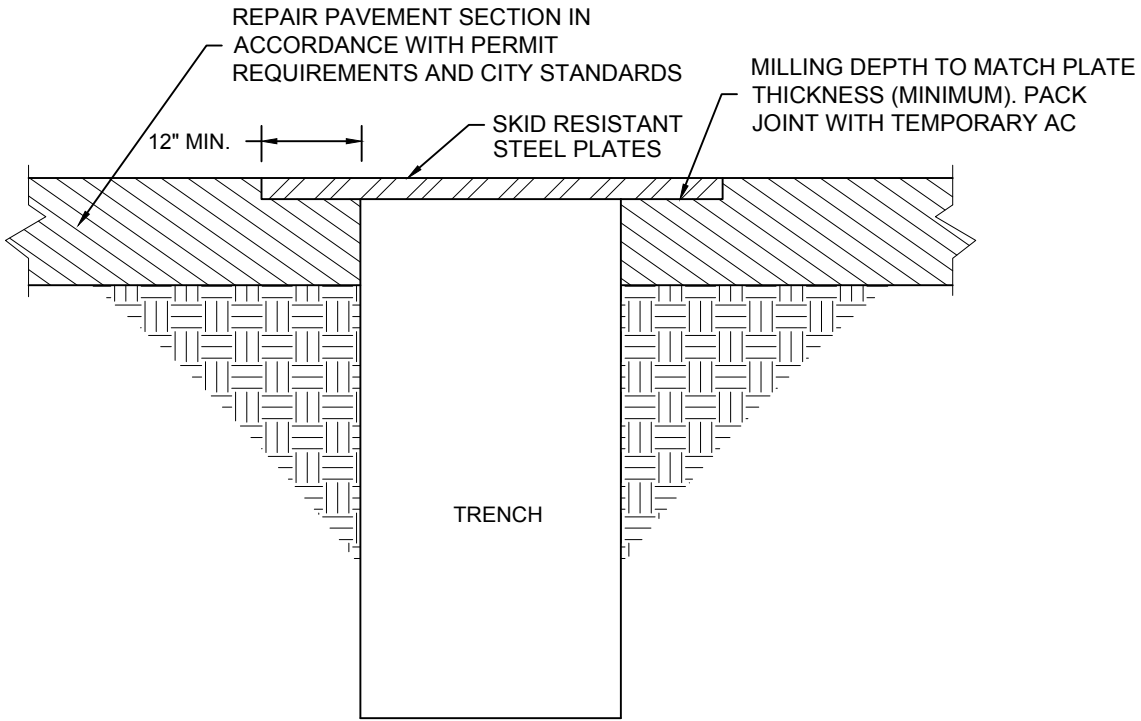
NARROW TRENCH  
BACKFILL AND ROADWAY  
REPAIR

STANDARD PLAN NO.

812

SHEET 4 OF 4

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**TYPICAL TRENCH PLATE DETAIL**  
N.T.S.

**NOTES**

1. A MINIMUM 12" LAP OF STEEL PLATE SHALL BE PROVIDED ON EACH SIDE OF TRENCH TO ASSURE NO SLIPPING OF PLATE OR COLLAPSING OF TRENCH WALL. WHERE 12" LAP CANNOT BE MET, ENGINEERING DESIGN IS REQUIRED AND SHALL BE APPROVED BY THE PUBLIC WORKS DIRECTOR. THE TRENCH SHALL BE ADEQUATELY SHORED IF NECESSARY TO SUPPORT THE BRIDGING AND TRAFFIC. FOR SPANS GREATER THAN FOUR FEET A SHORING PLAN AND A TRAFFIC CONTROL PLAN, ENGINEERED BY A REGISTERED CIVIL ENGINEER, SHALL BE SUBMITTED FOR REVIEW AND APPROVAL AT LEAST TWO WEEKS PRIOR TO START OF CONSTRUCTION WORK.
2. STEEL PLATE MUST FIT SNUG WITHIN THE RECESSED AREA AND INSTALLED TO OPERATE WITH MINIMUM NOISE.
3. THE PAVEMENT SHALL BE COLD PLANED TO A DEPTH EQUAL TO THE THICKNESS OF THE PLATE, AND TO A WIDTH AND LENGTH EQUAL TO THE DIMENSIONS OF THE PLATE.
4. THIS STANDARD SHALL BE IMPLEMENTED ON ALL PROJECTS WITHIN VEHICULAR TRAVEL WAY UNLESS OTHERWISE APPROVED BY THE PUBLIC WORKS DIRECTOR.
5. MULTIPLE PLATES MUST BE PINNED OR TACK WELDED AS NEEDED TO SECURE PLATES, 6" MINIMUM.
6. ALL PLATES MUST MEET REQUIRED TRAFFIC LOADS, AND BE SKID-RESISTANT, THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE APPROPRIATE SELECTION AND MAINTENANCE OF THE STEEL PLATES.
7. STEEL PLATES MUST BE REMOVED AND PERMANENT PAVEMENT SHALL BE PLACED WITHIN FOURTEEN (14) CALENDAR DAYS OR AS APPROVED BY THE PUBLIC WORKS DIRECTOR.
8. THE CONTRACTOR WILL BE REQUIRED TO PUT "STEEL PLATES AHEAD" WARNING SIGNS IN PLACE.

APPROVED BY:

*Jonathan George Smith*

2/5/2014

DIRECTOR OF PUBLIC WORKS  
JONATHAN GEORGE SMITH

DATE



**CITY OF MENIFEE**

**EMERGENCY PLATE  
BRIDGING FOR  
EXCAVATIONS**

STANDARD PLAN NO.

**813**

SHEET 1 OF 1

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